

## European Aviation Safety Agency

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AIR-40  
DER Seminar  
May 4, 2004

European Union

- Objectives
  - Promote European unity.
  - Promote economic development (balanced trade & fair competition) and social progress.
  - Introduce European citizenship.
  - Develop an area of freedom, security, and justice.
  - Maintain and build on established EU law.

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### Formation of Agency

- September 28, 2003 - EASA began operations in Brussels, Belgium
- EASA Management Board decisions
  - Executive Director, Mr. Patrick Goudou
  - Structure of Agency
  - Rulemaking process
- EASA HQ to move to Cologne, Germany (Sept. 04)

3

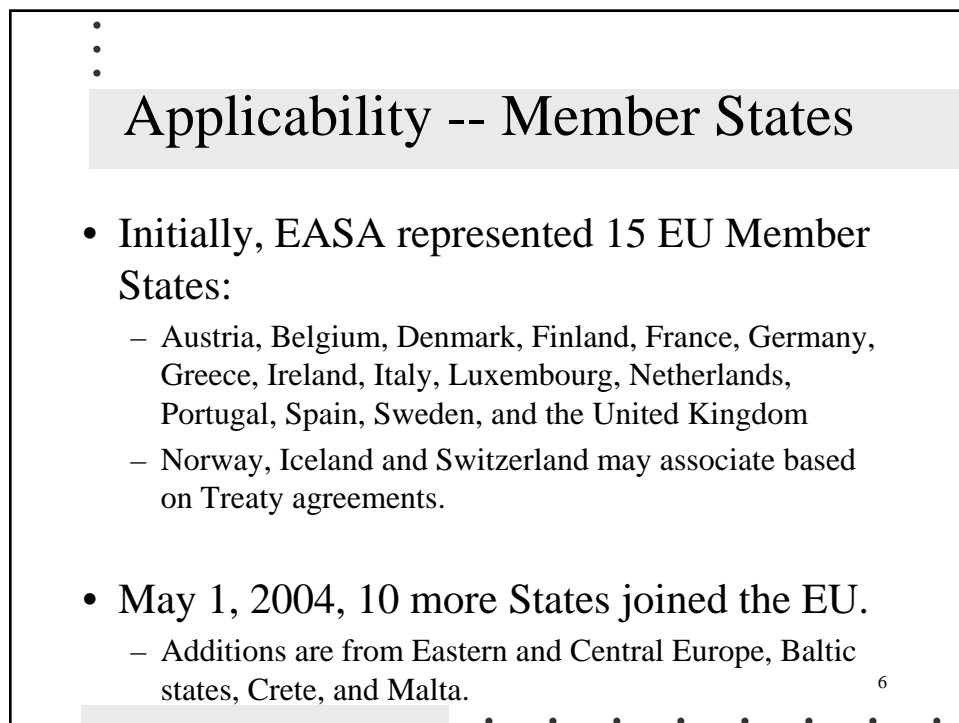
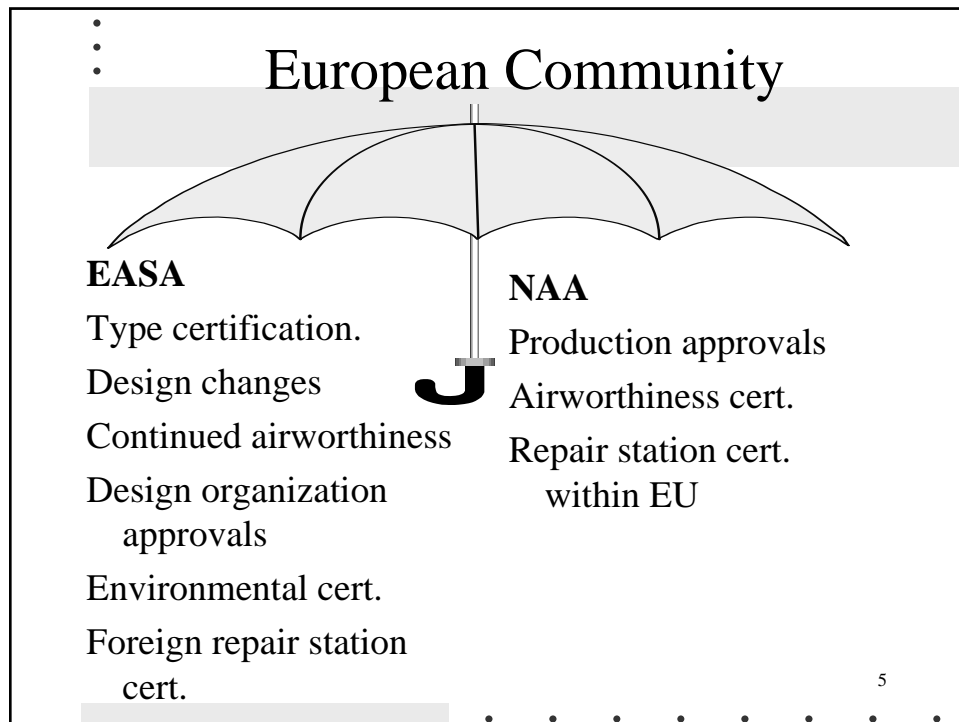
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### Local Administration under EASA Supervision

- Tasks of EASA are “outsourced” to qualified entities.
- National authorities are expected to issue:
  - Production Organization Approvals
  - Individual Certificates of Airworthiness
  - Repair station approvals within Europe

4

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### Continued Role of JAA

- Involved with transition
  - “core groups of experts”
- Represent non-EU members
  - 22 JAA countries

9

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### Bilateral Situation

- Currently, FAA has bilaterals with 11 EU member States for airworthiness and 3 for maintenance.
  - 6 BASA IPAs (Germany, France, U.K., Italy, Sweden, Netherlands)
- A new agreement must be negotiated with the European Union to replace all of the BAAs/BASA IPAs.

10

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### Bilateral Situation

- Existing bilaterals remain in effect until terminated by U.S. or partner
- Timing is critical issue
  - Need confidence in people, processes, and implementation

11

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### Status – AIR Team

- AIR EASA Team
  - Developed FAA Transition Order 8100.14 to provide guidance to AIR employees on how to handle projects, issues, etc.
  - Verified inventory of European products overseen by EASA vs. overseen by NAAs
  - Preparing for technical assessment of European system

12

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### FAA Technical Assessment

- Before concluding a new bilateral agreement, FAA requires a technical assessment of technical competence for each area of regulatory responsibility and oversight
- FAA has proposed a “joint” assessment covering both aircraft certification and maintenance functions.

13

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### FAA Technical Assessment

- 2-Stage assessment
  - Stage 1: Prior to December 2004
  - Stage 2: After December 2004 -- final assessment of EASA's operation
    - Using new EASA-led project(s) -- (1 TC, 1 STC)
    - Visiting non-bilateral/inactive NAAs
    - Possibly assess new EU Member States
- Outcomes:
  - Stage 1: Interim understanding with EU and bilateral partners.
  - Stage 2: Recommendations for new bilateral.

14

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### Expectations for Bilateral Process

#### – Why an Interim Understanding?

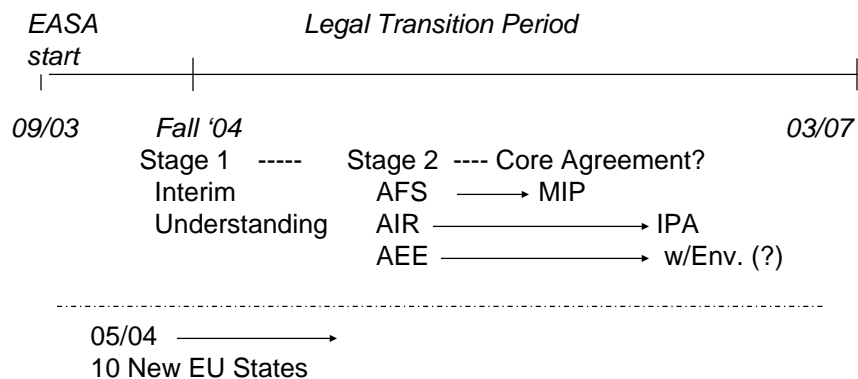
- Many EU decisions on EASA still pending
- Immediate operations will rely mostly on NAAs, not EASA itself
- Negotiations expected to be lengthy
- Need to have a framework to continue “business as usual” with Europe

15

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### Expectations for Bilateral Process

#### Timeline to Final Agreement Depends on EASA



16



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### Challenges

- Grandfathering concept--unilateral acceptance of all products registered in EU, design changes, repairs, etc. as of 9/2003.
- Assessing how EASA ramps up.
- EASA's standardization/qualification of new EU Member NAAs after May 2004.

17

### Transition Challenges

- Joint interim arrangements/transition plan
- Influencing development of EASA procedures, dissemination and application
- Changes needed to FAA policies, guidance
- Training
  - Employees, designees, industry/applicants

18

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### Summary

- DERs should be aware of the changes underway in Europe.
- Implementation of a single aviation agency in Europe will have far-reaching effects on current AIR operations.
- FAA must deal with EASA as **the** legally competent entity for all design, production, airworthiness and continued airworthiness issues in 25 EU Member States.

19

### Other Info Sources

#### – EU Documentation

- EU websites:  
[http://europa.eu.int/comm/transport/air/safety/index\\_en.htm](http://europa.eu.int/comm/transport/air/safety/index_en.htm)  
<http://www.easa.eu.int/>
- JAA certification:  
<http://www.jaa.nl/certification/certification.html>

#### – FAA Documentation

- EASA Frequently Asked Questions:  
<http://www.faa.gov/certification/aircraft/>

20

Atlantic City DER Recurrent Seminar – May 5, 2004

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### Other Info Sources

#### Industry Sources

- AIA: FAA Video from 09/02 & 09/03
- ([http://www.aiaaerospace.org/issues/subject/faa/easa\\_ivt.ppt](http://www.aiaaerospace.org/issues/subject/faa/easa_ivt.ppt))

#### FAA Video tapes available from OPI, Inc.

- Contact [melissa@opi2001.com](mailto:melissa@opi2001.com) or call 580-234-2845. Web site: [www.opi2001.com](http://www.opi2001.com).

21